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ADDENDUM No. ONE

Date: January 13, 2025

Project: Pembroke 2024 LMIG, MES No. 2024-15

Engineer: M.E. Sack Engineering
Hinesville, Georgia

The original plans, specifications, and bid documents are amended to include the following:

Bid Form:

- Replace the original Bid Form with the enclosed of the same. Note that Concrete Pavement Removal and 8" Granular Roadway Base have been removed, and the quantity and units have been revised for the 1.5" 9.5mm Asphalt, 2" Asphaltic Concrete Pavement, and $\frac{3}{4}$ " to 1" Crack Relief Interlayer.

Technical Specifications:

- Replace the original Section 01150 Measurement and Payment with the enclosed of the same. Note that Concrete Pavement Removal and Granular Roadway base have been removed, and the language regarding Asphalt Paving Overlay/Crack Relief Interlayer has been revised.

Plans:

- Replace the original sheets C4 and C5 with the enclosed of the same. Note the quantity estimates and language regarding Concrete Pavement Removal and 8" Granular Roadway Base have been revised on sheet C4, information specifically relating to milling depth for the section of roadway with no curb and gutter has been included on sheet C4, and the "Overlay Asphalt and Conform to Existing Curb and Gutter" detail on sheet C5 has been revised to provide

detailed direction on the milling and overlay procedure for the curbed portion of Burkhalter Street.

The following clarifications are offered for questions received:

1. *Can asphalt overlay be bid by tonnage as opposed to square yardage?*
 - Yes. The units for bid items 1.5" 9.5mm Asphalt, 2" Asphaltic Concrete, and ¾" to 1" Crack Relief Interlayer have been changed from square yardage to tonnage.
2. *Is milling through the concrete pavement patches within the roadway acceptable?*
 - Yes. The existing concrete pavement patches can be milled to the same depth as the surrounding asphalt pavement. Note that this will only apply to milling performed on Burkhalter Street (see Sheet C4). As a result, Concrete Pavement Removal and 8" Granular Roadway Base have been removed from the Bid Form.
3. *What is the depth of milling on Burkhalter Street?*
 - The milling depth along Burkhalter Street will vary depending on the true depth of the existing concrete gutter. Contractors will need to mill down to the original concrete gutters before milling and additional 1" below the top edge of the concrete gutter in the travel lane portion of the roadway. See Sheet C5 for full details on milling in these areas.
4. *It would be difficult to mill along the concrete gutter section without milling into the concrete of the gutter, is this acceptable?*
 - Yes, this would be acceptable; however, contractors will need to use a mini milling machine in these areas to reduce impacts made to the existing concrete sections.

-END-



B. BID FORM

Bid Item	Quantity	Units	Description	Unit Price	Cost
1	1	EA	Drainage Structure Removal	\$	\$
2	1	LS	Shoulder Scraping	-	\$
3	1	LS	Pavement Milling	-	\$
4	8	TN	1.5" 9.5mm Asphalt	\$	\$
5	3	TN	2" Asphaltic Concrete Pavement	\$	\$
6	5	TN	¾" to 1" Crack Relief Interlayer	\$	\$
7	22	LF	24" Stop Bar	\$	\$
8	892	LF	Double Solid Yellow Line Striping	\$	\$
9	4	EA	Adjust Water Valve Box Top	\$	\$
10	1	EA	Replace Drainage Grate	\$	\$
11	1	LS	Sholder Backing (Fill Material Placement)	-	\$
12	1	LS	Grassing	-	\$
13	1	LS	Traffic Control	-	\$
14	1	LS	Mobilization (5% Max)	-	\$
				TOTAL BID	\$

**SECTION 01150
MEASUREMENT AND PAYMENT**

PART 1 - GENERAL

1.01 QUANTITIES

- A. Quantities: Quantities listed in the Proposal are approximate only and are intended to serve as a guide in comparing bids, and may be increased or decreased without invalidating the unit price bid.
- B. Payment: Contractor shall be paid for actual in place quantities as determined by the Engineer field measurements.
- C. Discrepancies: In case of discrepancies between the figures shown in the unit prices and totals, the unit prices shall apply and the totals shall be corrected to agree with the unit price.

PART 2 - MEASUREMENT AND PAYMENT

2.01 STRUCTURE REMOVAL

- A. Measurement: Measurement shall be made on the basis of each item removed in accordance with the plans, specifications, and bid documents.
- B. Payment: Payment will be made on the basis of each structure removed from the site. The unit price bid shall include furnishing all labor, materials and equipment necessary to complete this item of work. Work shall include, but is not limited to, removal of any storm drainage structures, or other material that exist and will not be used as part of this project, excavation, disposal at an approved site, backfill, compaction and surface restoration.

2.02 SHOULDER SCRAPING

- A. Measurement: Measurement shall be made on the basis of the percentage complete of the task in accordance with the plans and specifications.
- B. Payment: Payment will be made at the lump sum stated in the bid. The price bid shall include furnishing all labor, materials, and equipment necessary to complete the work. Work shall include, but is not limited to, removal of all trees, shrubs, grass, soil, sand, and undergrowth that presently exist along the shoulder, preventing the construction of the project. All material removed including vegetation, roots and organic matter shall be removed from the site and disposed of at a permitted site. The contractor shall take special care not to disturb the roots of trees that are to remain. Trees to be saved shall be marked and approved by the engineer prior. Trees to be saved shall have the appropriate tree protection installed. Contractor shall be responsible for providing and replacing material removed in excess of what is specified on the plans.

2.03 PAVEMENT MILLING

- A. Measurement: Measurement shall be made on the basis of the completed item in accordance with the plans and specifications and accepted by the engineer.

- B. Payment: Payment will be made at the lump sum stated in the bid. The price bid shall include furnishing all labor, materials and equipment necessary to complete this item of work. Work shall include, but is not limited to, the milling of all asphaltic pavement as directed and the disposal of millings.

2.04 ASPHALT PAVING OVERLAY/CRACK RELIEF INTERLAYER

- A. Measurement: Measurement will be made on the basis of each ton of asphalt and/or interlayer in place, in accordance with the construction plans and accepted by the Engineer. Actual weight will be determined by weighing on a certified motor truck scale and presenting a notarized copy of the recorded weight to the Engineer.
- B. Payment: Payment shall be made on the basis of the number of tons of asphalt and/or interlayer in place in accordance with the unit price bid as stated in the contract. Work shall include, but is not limited to, the furnishing, hauling, placing and compaction of the asphalt in order to bring the pavement to the lines, grades, and cross sections as designated on the construction plans and as determined by the Engineer. The unit price bid shall also include surface cleaning, prime, tack and pavement markings. All striping will be in accordance with MUTCD and local specifications.

2.05 PAVEMENT MARKING

- A. Measurement: Measurement shall be made on the basis of each linear foot of pavement markings in place as per the plans and specifications.
- B. Payment: Payment shall be made at the linear footage stated in the bid. The unit price bid shall include all labor, materials, and equipment necessary to complete the task. The task shall include, but is not limited to, supplying, and installing all thermoplastic pavement markings to replace existing in accordance with construction plans, surface restoration and cleanup.

2.06 ADJUSTING TO GRADE OF MISCELLANEOUS ROADWAY STRUCTURES

- A. Measurement: Measurement will be made on the basis of adjusting each structure to grade, to determine the unit or units of each type completed and accepted, in accordance with the plans and specifications and accepted by the engineer. Structure tops to be raised or lowered 2 ft. (600 mm) or less are considered "Adjust to Grade."
- B. Payment: Payment will be made on the basis of each structure adjusted to grade. The unit price bid shall include all labor, materials and equipment necessary, including, but not limited to, excavation, shoring and sheeting, dewatering, gravel bedding, castings, backfill, compaction and complete surface restoration. Payment is full compensation for adjusting to grade the structures as specified in this Specification.

2.07 DRAINAGE STRUCTURE (Catch Basins, Yard Drain, Junction Box, Headwall, Concrete Flumes, Flared End Section, Pipe End Treatments, Interference Box, Outlet Structures)

- A. Measurement: Measurement will be made on the basis of each drainage structure installed at the elevation and location designated on the construction plans.

- B. Payment: Payment will be made on the basis of each structure installed at the unit price bid. The unit price bid shall include furnishing all labor, materials and necessary equipment to complete the item of work. Work shall include, but not be limited to, excavation, necessary shoring and sheeting, dewatering, forming, form wrecking, foundations as required, furnishing and installing the structure, placement of grates, manhole rings and covers as applicable, grouting around pipe, 6" stub for future underdrain connection, backfill, compaction, grading, complete surface restoration and cleanup.

2.08 SHOULDER BACKING

- A. Measurement: Measurement will be made on the basis of the percent complete of the item of work. All cut and fill quantities are based on the difference between initial topographic data and proposed contours shown on the plans.
- B. Payment: Payment will be made at the price bid for each item. Work shall include all equipment, labor and material to complete each task. This item will include, but is not limited to, excavation, material transportation and placement, grading to the lines and grades shown on the plans, compaction and stabilization.

2.09 GRASSING

- A. Measurement: Measurement shall be made on the basis of the completed item in accordance with the construction plans and bid items.
- B. Payment: Payment will be made in accordance with the price stated in the bid. The unit price shall include, but is not limited to, furnishing all labor, materials and equipment necessary for the satisfactory growth of grass on all disturbed areas in accordance with plans and specifications. Work shall include, but not be limited to, furnishing all materials, fertilizer, soil samples, grass seed, raking, leveling, watering, maintenance and final surface restoration. Final payment will not occur until permanent grass is established.

2.10 TRAFFIC CONTROL

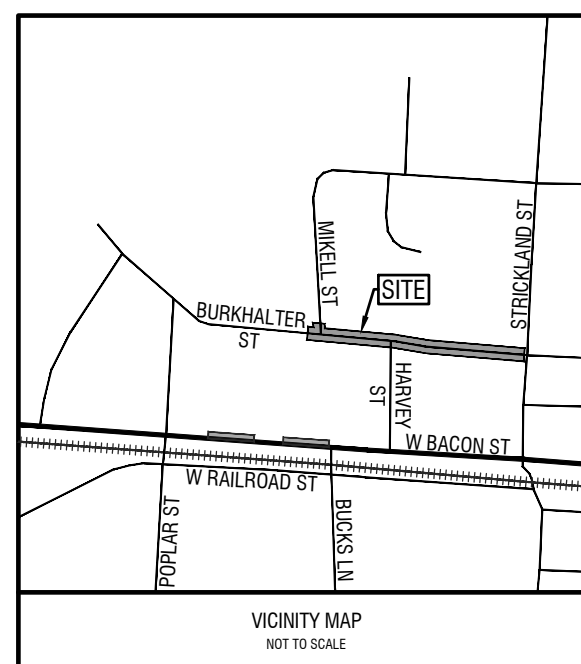
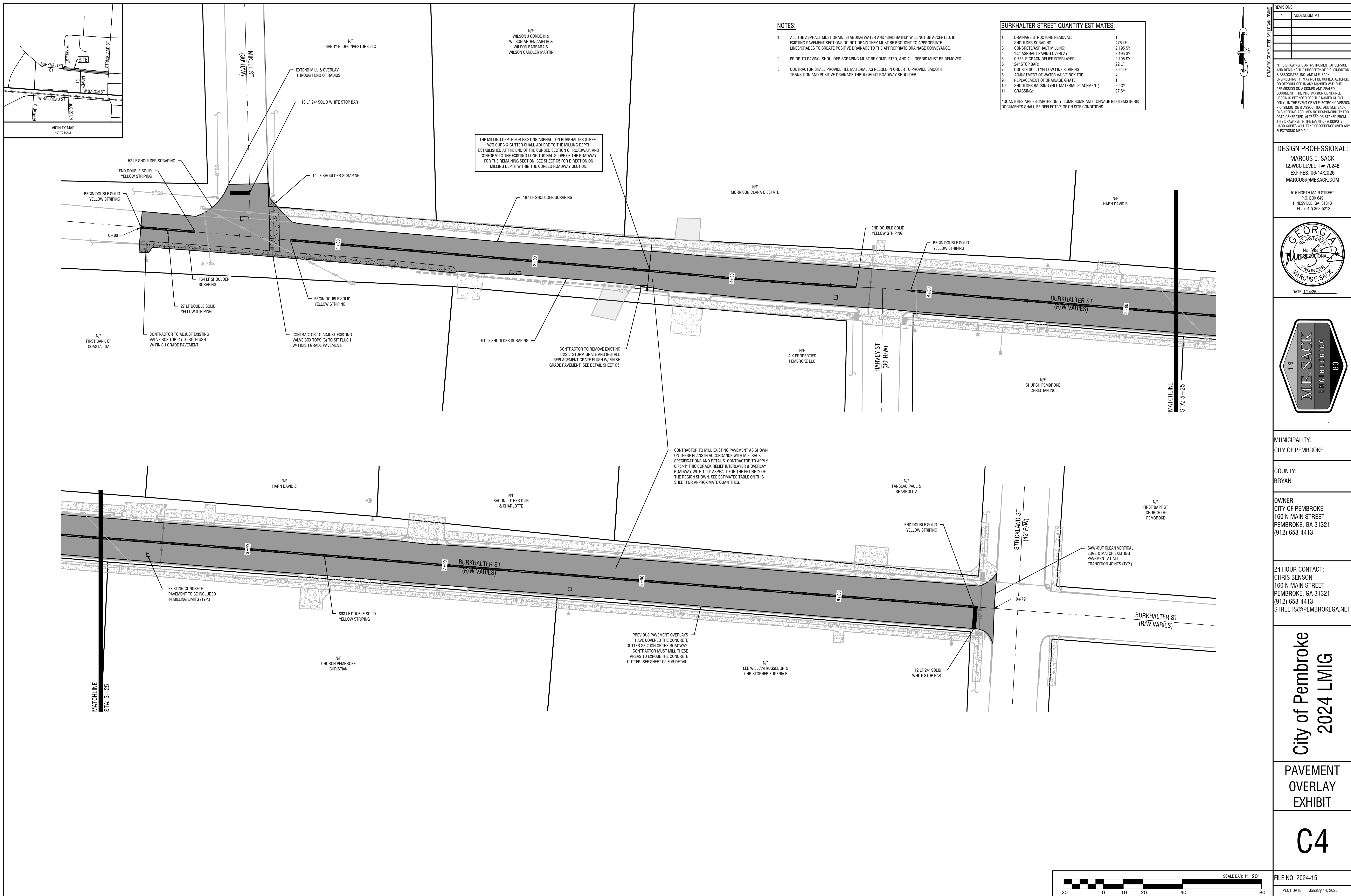
- A. Measurement: Measurement shall be made on the basis of the percentage complete of the lump sum bid in accordance with the construction plans and bid items.
- B. Payment: Payment shall be made on the basis of the percentage complete of the lump sum price stated in the bid as determined by the project engineer. The lump sum shall include furnishing all labor, materials, and equipment necessary to complete the task. The task shall include, but is not limited to, the placing, moving, and maintenance of all signage, barricades, cones, barrels, flagging, flag men, and guide vehicles throughout the construction process to safely reroute traffic from existing traffic patterns. Traffic control shall be done in a manner to safely warn, reroute, and lead vehicles to their destination. Additional signage will be required if the engineer deems that the traffic control in place does not fully meet the required intent of the task. Changing of existing traffic patterns shall be communicated with the engineer no less than 48 hours prior to.

2.11 MOBILIZATION

- A. Payment will be made for the price as stated in the Contract once the Contractor has established his construction yard, and met the requirements established in the

Contract Documents. Mobilization will be recognized complete once the Contractor has provided a construction schedule and moved his equipment and a substantial amount of material to the job site. Construction must be underway and progressing. Payment for mobilization will be limited to a maximum amount not to exceed 5% of the bid price.

END OF SECTION



- NOTES:**
1. ALL THE ASPHALT MUST DRAIN, STANDING WATER AND "BIRD BATHS" WILL NOT BE ACCEPTED. IF EXISTING PAVEMENT SECTIONS DO NOT DRAIN THEY MUST BE BROUGHT TO APPROPRIATE LINES/GRADES TO CREATE POSITIVE DRAINAGE TO THE APPROPRIATE DRAINAGE CONVEYANCE.
 2. PRIOR TO PAVING, SHOULDER SCRAPING MUST BE COMPLETED, AND ALL DEBRIS MUST BE REMOVED.
 3. CONTRACTOR SHALL PROVIDE FILL MATERIAL AS NEEDED IN ORDER TO PROVIDE SMOOTH TRANSITION AND POSITIVE DRAINAGE THROUGHOUT ROADWAY SHOULDER.

BURKHALTER STREET QUANTITY ESTIMATES:

1. DRAINAGE STRUCTURE REMOVAL:	1
2. SHOULDER SCRAPING:	476 LF
3. CONCRETE/ASPHALT MILLING:	2,195 SY
4. 1.5\"/>	

REVISIONS:

1.	ADDENDUM #1

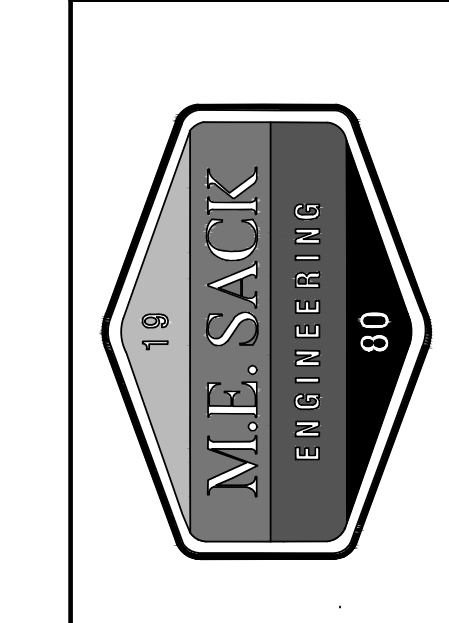
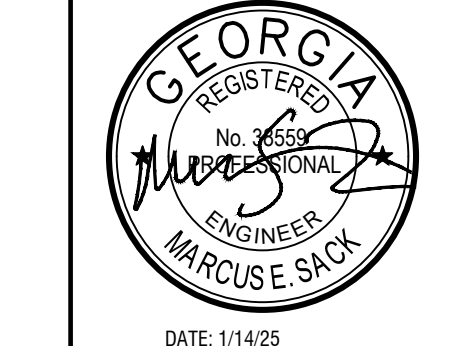
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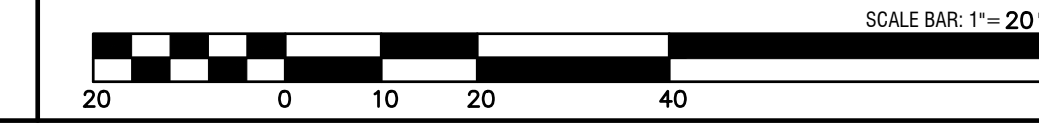
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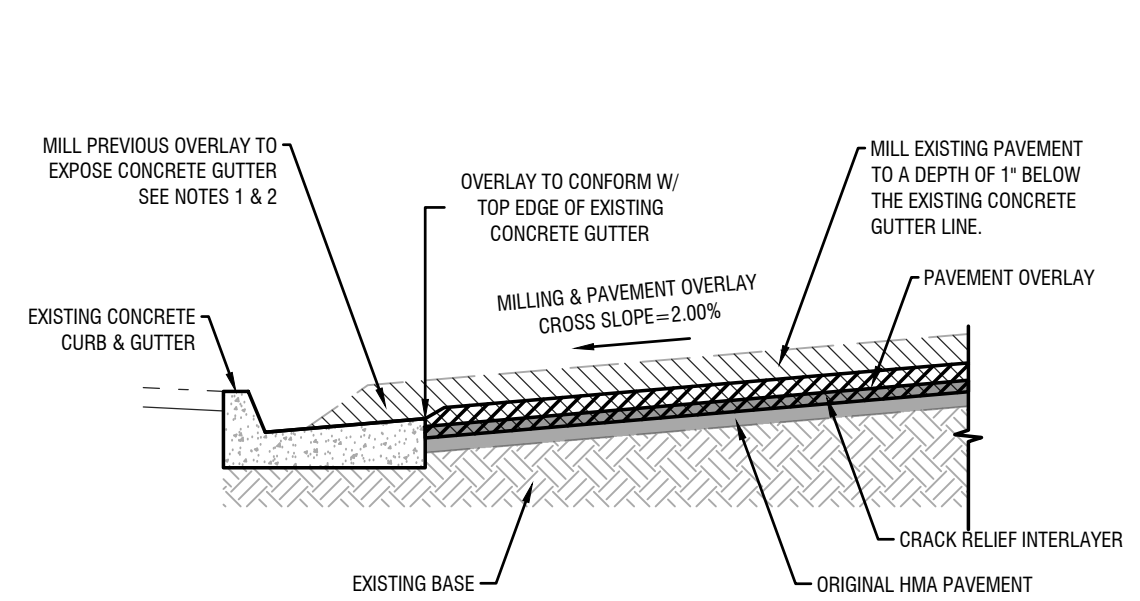
City of Pembroke
2024 LMIG

PAVEMENT OVERLAY EXHIBIT

C4

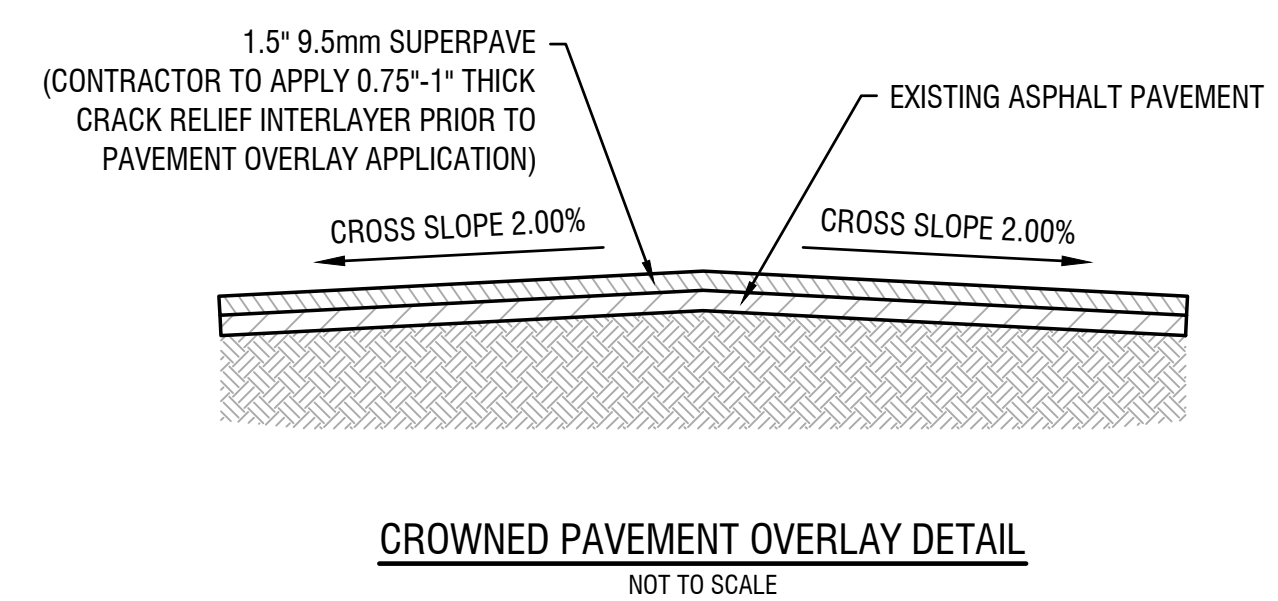
FILE NO: 2024-15
 PLOT DATE: January 14, 2025



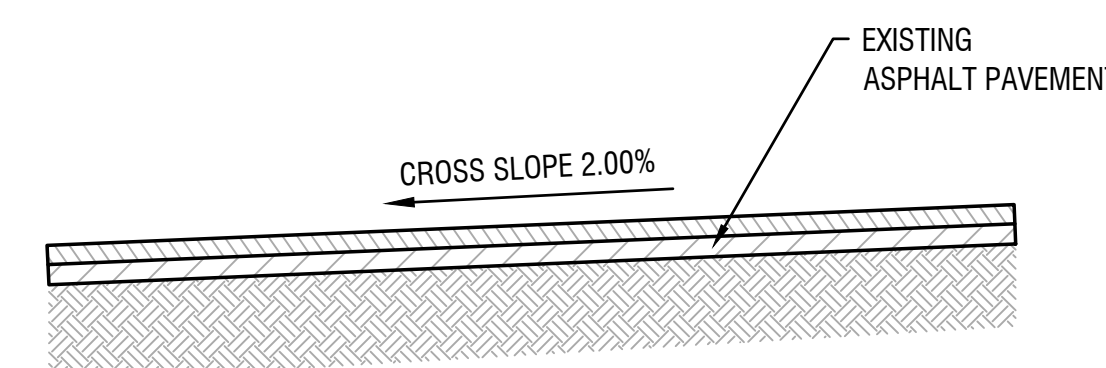


- NOTES:**
1. CONTRACTOR TO MILL EXISTING PAVEMENT DOWN TO THE DEPTH OF THE ORIGINAL CONCRETE GUTTER SECTION, FOLLOWED BY ADDITIONAL MILLING OF THE TRAVEL LANE TO A DEPTH OF 1" BELOW THE TOP EDGE OF THE EXISTING CONCRETE GUTTER LINE.
 2. CONTRACTOR SHALL USE MINI MILLING MACHINE WHEN MILLING ABOVE/AROUND THE CONCRETE GUTTER SECTIONS TO MINIMIZE IMPACT TO THE ORIGINAL CONCRETE.
 3. PROPOSED OVERLAY IS TO BE APPLIED WITH A 2.00% CROSS SLOPE. EXISTING PAVEMENT IS TO BE MILLED AT THE EQUIVALENT CROSS SLOPE TO ALLOW FOR CONSISTENT OVERLAY APPLICATION.
 4. SEE OVERLAY DETAILS FOR TYPE OF PAVEMENT & OVERLAY DEPTH REQUIRED.

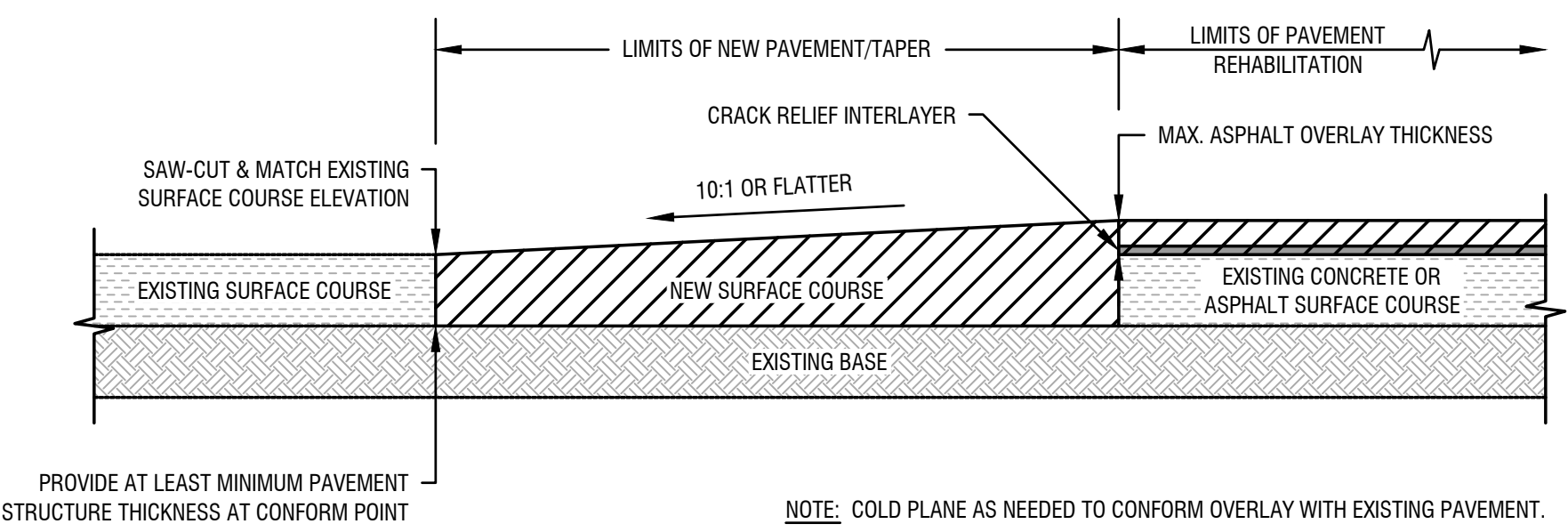
OVERLAY ASPHALT CONFORM TO EXISTING CURB & GUTTER DETAIL
NOT TO SCALE



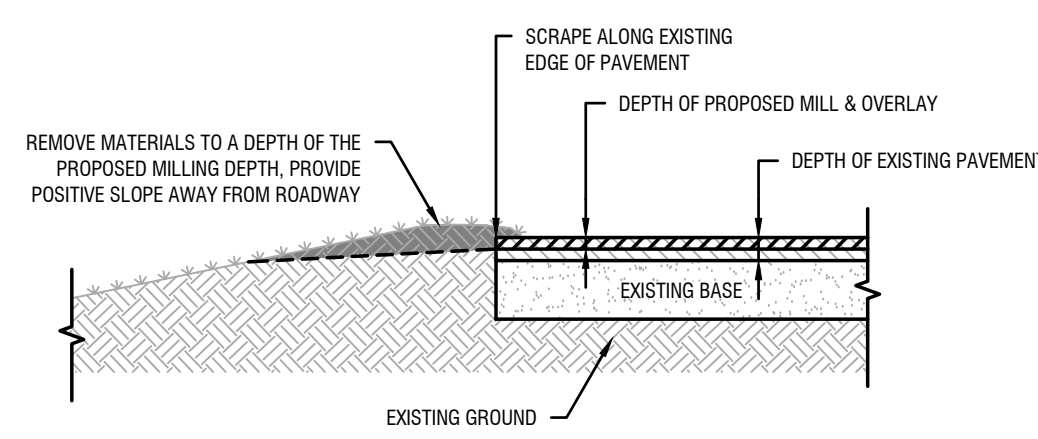
CROWNED PAVEMENT OVERLAY DETAIL
NOT TO SCALE



SUPER ELEVATED PAVEMENT OVERLAY DETAIL
NOT TO SCALE

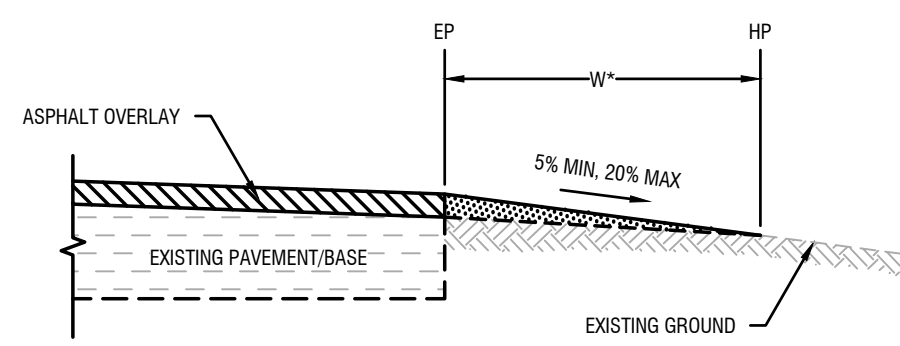


OVERLAY PAVEMENT TRANSITION JOINT DETAIL
NOT TO SCALE

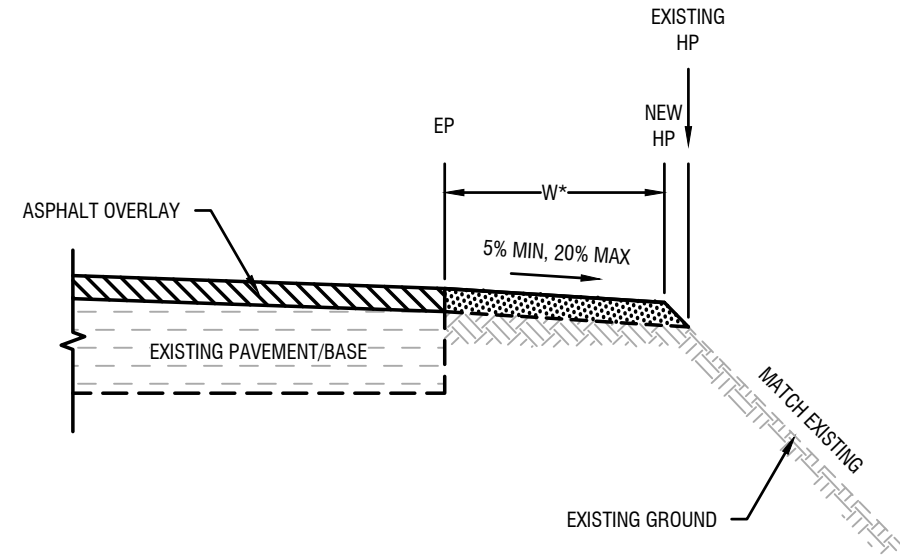


- NOTES:**
1. CONTRACTOR TO SCRAPE A CLEAN VERTICAL CUT ALONG THE EDGE OF EXISTING PAVEMENT & REMOVE ANY MATERIAL PREVENTING PROPER DRAINAGE AWAY FROM THE PROPOSED ROADWAY SURFACE.
 2. SHOULDERS MUST BE SCRAPED PRIOR TO PAVING AND REMAIN CLEAR OF DEBRIS UNTIL THE FINAL PAVEMENT APPLICATION AND SHOULDER BACKING HAS BEEN COMPLETED.
 3. SHOULDERS MUST BE SCRAPED AS NEEDED TO ENSURE BACKING WILL PROVIDE POSITIVE DRAINAGE & CONFORM TO THE OVERALL DRAINAGE CONVEYANCE OF THE EXISTING SITE. REFER TO SHOULDER BACKING REQUIREMENTS TO ENSURE ADEQUATE SCRAPING WIDTH/SLOPE.
 2. CONTRACTOR IS RESPONSIBLE FOR PROVIDING ANY FILL MATERIAL NEEDED TO CONSTRUCT/ RECONSTRUCT ROADWAY SHOULDER ONCE PROPOSED PAVEMENT OVERLAY HAS BEEN APPLIED. SHOULDERS MAY BE CONSTRUCTED USING IN SITU MATERIAL, AS LONG AS MATERIAL MEETS THE REQUIREMENTS DESCRIBED IN THE SHOULDER BACKING DETAIL ON THIS SHEET.

SHOULDER SCRAPING DETAIL
N.T.S.



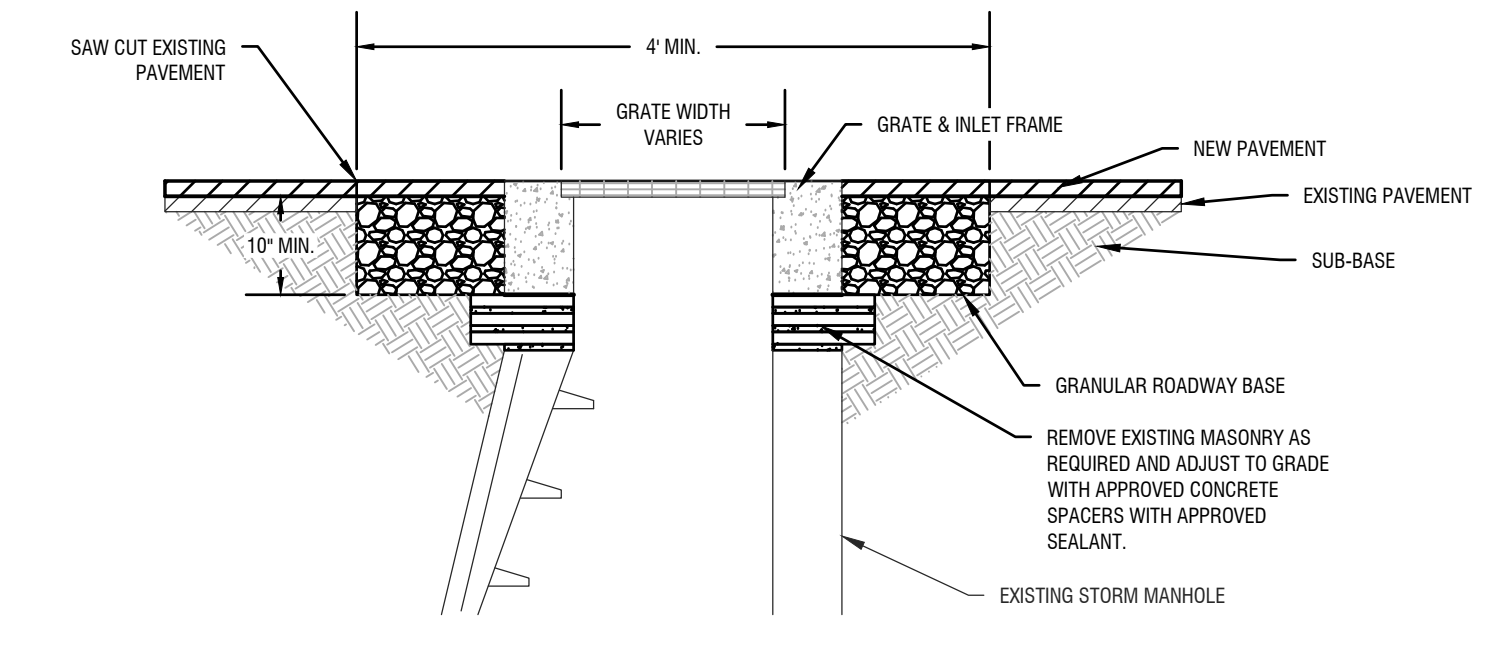
TYPICAL (MOST COMMON) APPLICATION



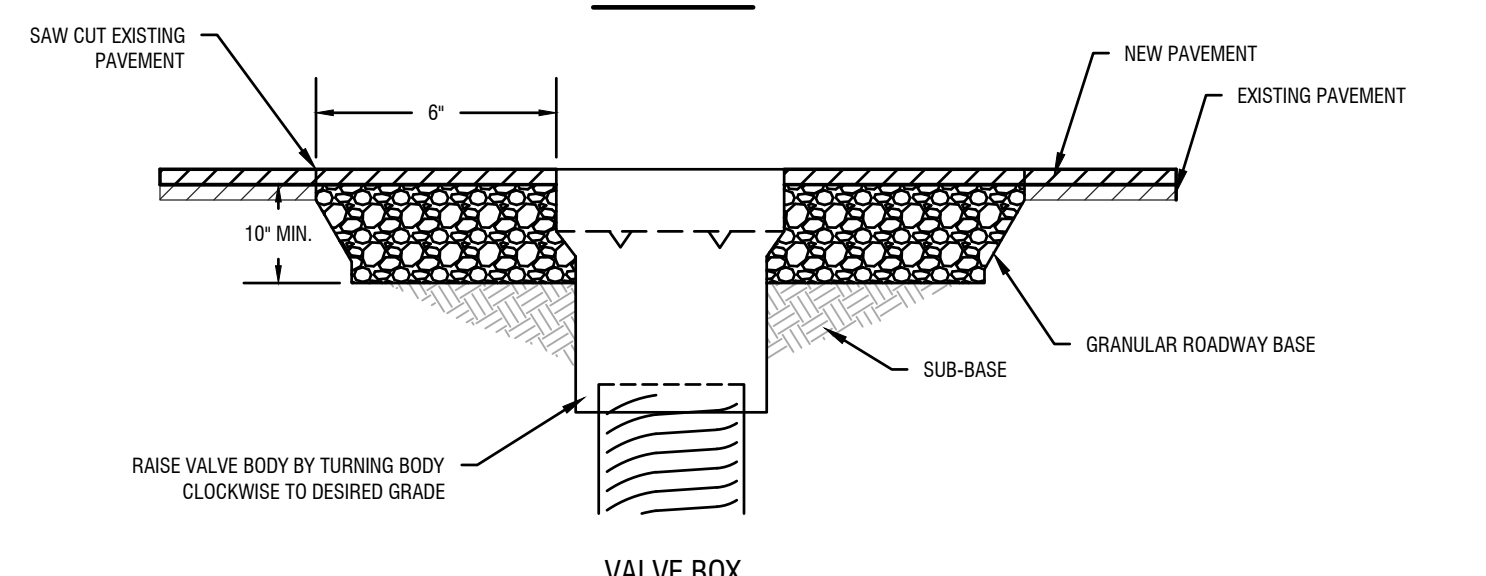
ALTERNATIVE PLACEMENT FOR EXISTING SLOPES STEEPER THAN 6:1

*WIDTH REQUIREMENTS:
MIN. WIDTH = 2 FT
MAX. WIDTH = 3 FT (WHERE SITE CONDITIONS ALLOW)
WIDTH REQUIREMENTS ALSO EXIST FOR DIKES, MGBR, AND CONCRETE BARRIERS.
MATERIAL: SHOULDER BACKING MATERIAL BE GRANULAR BASE MATERIAL COMPACTED TO 95% PROCTOR.

SHOULDER BACKING DETAIL
N.T.S.



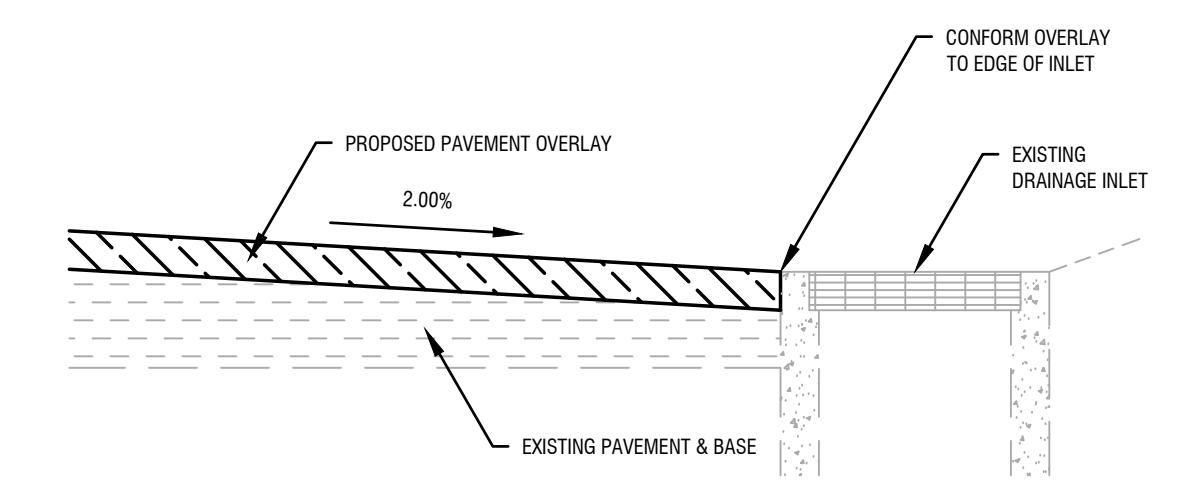
GRATE INLET



VALVE BOX

- NOTE:**
1. STRUCTURES SHALL NOT BE ADJUSTED FOR A PERIOD OF 24 HOURS AFTER RESURFACING IS COMPLETED IN THAT AREA.
 2. ASPHALT SHALL BE SAWCUT TO MAKE A SMOOTH, EVEN EDGE.
 3. STRUCTURE SHALL BE ADJUSTED TO FIT FLUSH WITH STREET SURFACE.
 4. USE AT LEAST 3/4\"/>

STRUCTURE ADJUSTMENT IN TRAFFIC AREAS
N.T.S.



ASPHALT OVERLAY CONFORM TO EXISTING DRAINAGE
NOT TO SCALE

NOTE: DETAILS ON THIS SHEET ONLY APPLY TO THE BURKHALTER STREET SITE (SHEET C4). REFER TO SHEETS C1-C3 FOR PAVEMENT DETAILS COVERING THE STATE ROADWAY PAVEMENT OVERLAYS.

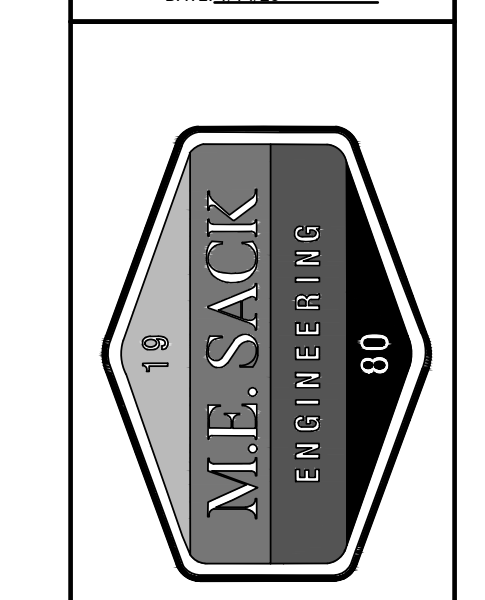
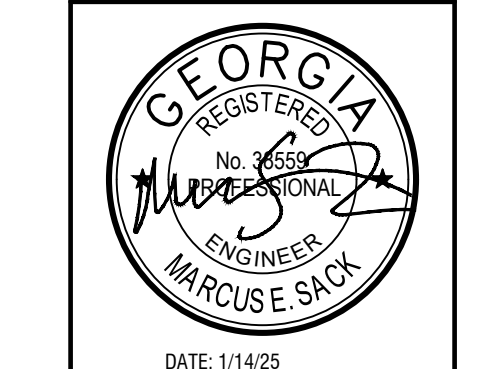
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REVISIONS:
1. ADDENDUM #1

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City of Pembroke
2024 LMIG

DETAILS

C5

FILE NO: 2024-15
PLOT DATE: January 14, 2025